



ACTIVE TRANSPORTATION AND TRANSPORTATION MASTER PLANS

APPENDIX D DETAILED CYCLE ROUTE ASSESSMENT





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1 INTRODUCTION

This appendix summarizes the detailed assessment completed to develop the proposed cycling network for the Township of Scugog illustrated on **Map 9 (Rural Area)** and **Map 10 (Port Perry Urban Area)** of the **Active Transportation and Transportation Master Plans (AT and TMP)**. The document should be read in conjunction with Subsection 4.3.3 Network Development of the plans.

2 PROPOSED ROUTES

This section details the assessments completed to identify the primary east-west and north-south cycling routes per Step 3 of the *Cycling Network Development Process* illustrated in **Figure 4.3** of the AT and ATP. Separate assessments were completed for the Port Perry Urban Area and the rural area of the Township.

2.1 PORT PERRY URBAN AREA

The following describes the proposed cycling routes in the Port Perry Urban Area listed in **Table 4.2** and **Map 9** of the AT and TMP:

2.1.1 EAST-WEST ROUTES

King Street

King Street is a Type C Arterial road situated at the southern limit of Port Perry. In the west, King Street terminates at the junction of Rose Street and Highways 7A and 12. In the east, King Street intersects with Simcoe Street and becomes Oyler Drive, a Local road, to the east and terminates.

Within the Port Perry Urban Area, King Street is a two-lane roadway with a posted speed limit of 50 kilometres per hour and a relatively wide right-of-way. Vehicular volumes along the roadway vary between 1,700 and 2,400 vehicles per day. Where King Street passes through the Prince Albert community, there is a sidewalk with an asphalt boulevard on the north side and a narrow asphalt boulevard on the south side. Development is underway at the eastern end of the road, with future growth planned west of Simcoe Street.

While there are currently not many places of interest along the route, it connects to several other planned routes and the future development in the area may offer more attractions. Also, King Street is relatively flat and provides an attractive and enjoyable ride for cyclists.



Due to the higher vehicular volumes along King Street and the expected growth in the area, it is important that any new cycling infrastructure be separated from vehicular traffic. Suggested improvements include constructing a multi-use path on the north side of the roadway from Old Simcoe Road to Simcoe Street. When the intersection of King Street and Simcoe Street is reconstructed, the route should be extended west with future development. A signalized crossride should also be considered at the intersection.

Recognizing implementation could take time, the Township should begin by posting on-road bike route signs between Old Simcoe Road and Simcoe Street in the interim.

Victoria Street and Earl Cuddie Boulevard

Victoria Street is a low speed, low volume Collector road connecting residential communities in south Port Perry. It currently terminates in the west at Alma Street, but with future development could extend to Highway 7A at the signalized intersection opposite the Smart Centres development. This connection would be advantageous to cyclists wishing to travel to the commercial establishments on the west side of Highway 7A that currently must use Scugog Street for this purpose.

East of Union Avenue, Victoria Street transitions into Earl Cuddie Boulevard, a Local road that leads to both commercial and recreational uses closer to Simcoe Street. While coordination with Durham Region is required to facilitate cycling infrastructure on Simcoe Street beyond Herbert A. Bruce Park, this connection would greatly enhance the overall bike network.

The potential connections to commercial development, the relatively low traffic volumes (<1,000 vehicles per day), and connections to other routes make the Victoria Street (Highway 7A to Union Avenue) and Earl Cuddie Boulevard (Union Avenue to Dr. Herbert A. Bruce Park) route a preferred cycling corridor and a candidate for a *Bicycle Priority Street*. OTM Book 18 indicates this type of facility is an appropriate solution for streets with low volumes and speeds. Designed to offer priority for cyclists operating in mixed traffic, a Bicycle Priority Street combines pavement markings, traffic calming measures, and crossing improvements to enhance the cycling experience. **Figure D.1** illustrates a sample design.

Recognizing implementation could take time, the Township should begin by posting on-road bike route signs in the interim along with wayfinding signs indicating the direction cyclists should travel to reach specific destinations (such as Prince Albert, Walmart or downtown). A multi-use trail should also be contemplated for the west portion of the route to connect through to Highway 7A before Victoria Street is extended.



FIGURE D.1: BICYCLE PRIORITY STREET

(Source: *Small Town and Rural Multimodal Networks*, Figure 2.5, p. 2-11)

Options to connect from Dr. Herbert A. Bruce Community Park to Simcoe Street, with the intent to eventually link to Lakeview Drive, were considered to ensure continuity of the proposed cycling network. **Figure D.2** illustrates three options considered to create the connection. **Table D.1** summarizes the assessment of the alternatives. Based on the analysis, Option 3 was identified as the preferred solution. This option involves:

- Paving unpaved sections of the multi-use trail already within Dr. Herbert A. Bruce Community Park;
- Coordinating with Durham Region to construct a multi-use path on the west side of Simcoe Street from the park to Vanedward Drive; and
- Installing a crossroad on Simcoe Street at Vanedward Drive.

Lakeview Drive and Carnegie Street

Lakeview Drive and Carnegie Street are Local roads carrying relatively low vehicular traffic volumes (<1,000 vehicles per day). While relatively lightly travelled at most times, the streets can serve as a cut-through route for motorists attempting to avoid the busy Simcoe Street and Scugog Street intersection.

Providing cycling infrastructure on Lakeview Drive and Carnegie Street between Simcoe Street and Scugog Street could help to slow shortcutting motorists and potentially discourage drivers from using the route. In the longer term, the roads should be considered for Bicycle Priority Street treatment. A signed on-road bike route should be installed as an interim measure.



FIGURE D.2: OPTIONS TO CONNECT HERBERT A. BRUCE PARK TO SIMCOE STREET

The unsignalized intersections at both ends of the route could present challenges to cyclists. Both intersect high-volume roads (Simcoe Street and Scugog Street) and are located too close to existing signalized intersections to allow for additional traffic control signals. To connect to nearby routes, cyclists should be directed to the closest signalized intersections, Simcoe Street and Vanedward Drive and Scugog Street and Water Street, using wayfinding signs. Section 3 presents options for linking the Lakeview Drive end to the Victoria Street and Earl Cuddie Boulevard route and the Carnegie Street terminal to the Waterfront Trail.

Scugog Street

From Queen Street/Scugog Line 6 to Water Street, Scugog Street (Highway 7A) is a two/three-lane urban Type A Arterial road. The road carries a high volume of traffic, including many trucks, owing to its important function as the main gateway to Port Perry from the east and west. In this role, Scugog Street connects to many places of interest in the Township.

The proposed Regional Cycling Plan Update denotes a future facility on Scugog Street. Given the higher volumes of vehicular traffic, a boulevard multi-use path should be considered for the east side. The path would replace sections of existing sidewalk.



TABLE D.1: ASSESSMENT OF OPTIONS TO CONNECT HERBERT A. BRUCE PARK TO SIMCOE STREET

Option	Description	Opportunities	Challenges
<u>Option 1</u> Earl Cuddie Boulevard to Major Street to Alva Street	<ul style="list-style-type: none"> Install wayfinding signs along route 	<ul style="list-style-type: none"> Uses low volume, low speed roadways Does not require new multi-use path on Simcoe Street 	<ul style="list-style-type: none"> Protected crossing cannot be installed at Lakeview Drive and Simcoe Street due to proximity of nearby signals
<u>Option 2</u> Earl Cuddie Boulevard to Major Street to Simcoe Street	<ul style="list-style-type: none"> Install wayfinding signs along route Construct boulevard multi-use path on west side of Simcoe Street from Major Street to Lakeview Drive 	<ul style="list-style-type: none"> Uses some low volume, low speed roadways Separates cyclists from vehicular traffic for part, thereby improving safety and comfort More direct route than Option 1 	<ul style="list-style-type: none"> Protected crossing cannot be installed at Lakeview Drive and Simcoe Street due to proximity of nearby signals Coordination with Durham Region on type and timing of facility on Simcoe Street, which may not be consistent with broader plan
<u>Option 3</u> Herbert A. Bruce Community Park to Simcoe Street PREFERRED	<ul style="list-style-type: none"> Pave unpaved sections of off-road multi-use trail in park to Simcoe Street Construct boulevard multi-use path on west side of Simcoe Street from park to Vanedward Drive and on east side from Vanedward Drive to Lakeview Drive Install signalized crossride on Simcoe Street at Vanedward Drive Install wayfinding signs along route 	<ul style="list-style-type: none"> Leverages existing signalized crossing on Simcoe Street Connects to planned Regional infrastructure on Simcoe Street Separates cyclists from motor vehicles, thereby improving safety and comfort Connects to retail land uses Improves connectivity for pedestrians More direct route than Option 1 	<ul style="list-style-type: none"> Coordination required with Durham Region on type and timing of facility on Simcoe Street Most expensive option



Scugog Street

From Queen Street/Scugog Line 6 to Water Street, Scugog Street (Highway 7A) is a two/three-lane urban Type A Arterial road. The road carries a high volume of traffic, including many trucks, owing to its important function as the main gateway to Port Perry from the east and west. In this role, Scugog Street connects to many places of interest in the Township.

The proposed Regional Cycling Plan Update denotes a future facility on Scugog Street. Given the higher volumes of vehicular traffic, a boulevard multi-use path should be considered for the east side of the road. The path would replace sections of existing sidewalk.

Queen Street

Queen Street is a Collector road carrying a moderate volume of vehicular traffic. Relatively narrow and with a grade down towards Lake Scugog, the street connects to several places of interest including downtown Port Perry and the waterfront. It also provides access to R.H. Cornish Public School and Port Perry High School.

This section of Queen Street is identified on the Provincial CycleON Network and Regional Primary Cycling Network as a cycling route, thereby necessitating some form of recognition in the AT and TMP. Given its narrow platform, the Township should begin by posting on-road bike route signs pending the outcome of the proposed corridor operation and design study.

The Scugog Line 6/Queen Street/Scugog Street intersection is a matter of concern with the subject section. The complicated geometry of the intersection increases the complexity of crossing manoeuvres. A signalized crossroad should be considered to enhance safety and improve guidance for cyclists.

Paxton Street and Perry Street

Paxton Street and Perry Street are Local roads connecting Lakeridge Health Port Perry to Queen Street and downtown Port Perry. Relatively wide and straight, Paxton Street was designed to serve emergency vehicle access to the hospital but experiences greater incidence of speeding by other motorists because of its design. Perry Street features somewhat similar design characteristics and traffic patterns, serving as a key access point to downtown Port Perry. The absence of a traffic control device at the Simcoe Street and Paxton Street intersection hinders cyclists wishing to cross at this location and continuity of the route.



Paxton Street from Old Simcoe Road to Perry Street and Perry Street from Paxton Street to Queen Street are candidates for Bicycle Priority Street treatment. Caution should be taken to ensure the traffic calming measures introduced do not impede emergency vehicles accessing the hospital. A Pedestrian Crossover should also be considered at the intersection of Simcoe Street with Paxton Street to allow cyclists and pedestrians to safely cross this major roadway.

Recognizing implementation could take time, the Township should begin by posting on-road bike route signs in the interim.

Reach Street and Coulter Street

Reach Street (Regional Road 8) is a Type B Arterial road under the jurisdiction of Durham Region. Coulter Street is a Local road.

A multi-use path currently exists on the south side of Reach Street from Simcoe Street to Old Simcoe Road. The section of trail from Simcoe Street to Bigelow Street was recently upgraded, but the remaining portion west of Bigelow Street is narrow and uneven with low hanging tree branches as **Figure D.3** shows. There are several places of interest along this corridor including seniors' residences, the Port Perry Fairgrounds, bus stops, places of worship, and commercial development.



**FIGURE D.3: MULTI-USE PATH ON
REACH STREET WEST OF BIGELOW STREET**

The trees on Reach Street should be trimmed to provide an overhead clearance of 2.1 metres per the Integrated Accessibility Standards of the *Accessibility for Ontarians with Disabilities Act*.

The existing multi-use path between Old Simcoe Road and Bigelow Street should also be repaved and widened to provide a surface more suitable to both cyclists and pedestrians with crossrides contemplated at Old Simcoe Road (signalized) and Bigelow Street (unsignalized).

East of Simcoe Street, Reach Street turns into Coulter Street and connects to other existing and proposed cycling routes. Coulter Street should be considered for a Bicycle Priority Street treatment between Simcoe Street and the Waterfront Trail as the connecting east-west corridor with other routes. Recognizing implementation could take time, the Township should begin by posting on-road bike route signs in the interim.



2.1.2 NORTH-SOUTH ROUTES

Old Simcoe Road and Scugog Line 8

Within the Port Perry Urban Area, Old Simcoe Road is a two-lane Collector road with a posted speed limit of 50 kilometers per hour. Higher vehicular volumes (~5,000 vehicles per day) are experienced on Old Simcoe Road north of Queen Street, while lower volumes occur south of Queen Street (~2,500 vehicles per day).

Old Simcoe Road is a preferred north-south route for the cycling network. It provides a direct connection through Port Perry with access to many amenities and communities along its length. The wider roadway width and its intersections with other recommended cycling routes also make the corridor convenient and desirable. Additionally, Durham Region has indicated in the proposed Regional Cycling Plan Update that cycling infrastructure should be planned on Old Simcoe Road.

On this basis, “urban shoulders” or separated or buffered bike lanes should be installed on Old Simcoe Road to create a continuous cycling corridor over its length from King Street to Edinborough Avenue. A crossside should also be considered at the intersection with Scugog Street. The Township should begin by posting on-road bike route signs over the entire route and marking “urban shoulders” between King Street and Edinborough Avenue where feasible.

Bigelow Street

Bigelow Street is a two-lane Local road that parallels Simcoe Street, stretching from Reach Street to Queen Street. The route provides connections to the Reach Street multi-use path, the proposed Bicycle Priority Street on Paxton Street, and future potential cycling facilities on Queen Street. Average daily volumes on Bigelow Street are low (250 to 300 vehicles per day) and the adjacent development is almost exclusively residential. These characteristics lend themselves to a desirable cycling connection.

On this basis, Bigelow Street, from Reach Street to Queen Street, should be considered for Bicycle Priority Street treatment. The Township should begin by posting on-road bike route signs over the entire length and marking “urban shoulders” between Queen Street to Kellett Street in the interim recognizing implementation could take time.

South of Queen Street, Bigelow Street becomes Crandell Street, which continues to Scugog Street. There is no signalized or protected crossing at the Bigelow Street and Scugog Street intersection, nor does the road section connect to any protected cycling infrastructure. Fire Station One is also located on Crandell Street. Combined, these factors suggest this section is not a route candidate at the current time but may be reconsidered in the future should cycling facilities be installed on Scugog Street.



Union Avenue and Lorne Street

Union Avenue is a two-lane Collector road that stretches diagonally from King Street to Lorne Street in south Port Perry. The road connects several residential neighbourhoods. Average traffic volumes along the route range from 800 to 1100 vehicles per day.

The Township should begin by posting on-road bike route signs along Union Avenue. Urban shoulders are currently provided on both sides of Union Avenue from Josephine Street to Major Street, as **Figure D.4** shows. Near term construction plans include providing bike lanes to King Street. The Township should construct and mark these features as bike lanes where feasible. The remaining section of Union Avenue from Major Street to Lorne Street is also contemplated for reconstruction, although not in the immediate term. Bike lanes should be included in the project.



**FIGURE D.4: UNION AVENUE
URBAN SHOULDER**

At the north termination of Union Avenue, Lorne Street is a Local road connecting to Simcoe Street. Two options were considered to connect the north end of Union Avenue via Lorne Street to the proposed cycling network as illustrated in **Figure D.5**. Based on the assessment completed, Option 2, east on Lorne Street, is preferred.

Simcoe Street (south of Scugog Street)

From north of King Street to Scugog Street, Simcoe Street (Regional Road 2) is a three/four-lane urban Type A Arterial road. The road carries a high volume of traffic, including many trucks, owing to its important function as the main gateway to Port Perry from the south. In this role, Simcoe Street connects to many places of interest in the Township.

The proposed Regional Cycling Plan Update denotes a future facility on Simcoe Street. Given the higher volumes of vehicular traffic, a boulevard multi-use path should be considered for the east side of the road. The path would replace sections of existing sidewalk.



FIGURE D.5: OPTIONS TO CONNECT UNION AVENUE

Water Street

Water Street is a Collector road carrying a moderate volume of vehicular traffic through downtown Port Perry. On-street parking is permitted on both sides of the road, with sections on the east side operating with angled parking. With its prominent location adjacent to Lake Scugog, the street connects several places of interest including downtown Port Perry, the waterfront, and commercial and institutional land uses.

This section of Water Street is identified on the Provincial CycleON Network and Regional Primary Cycling Network as a cycling route, thereby necessitating some form of recognition in the AT and TMP. Given the presence of on-street parking, the Township should post on-road bike route signs.

Sherrington Drive and Chimney Hill Way

Sherrington Drive and Chimney Hill Way are low speed, low volume Local roads connecting between Reach Street to Old Simcoe Road in the Cawkers Creek subdivision. The roads are abutted solely by residential uses and provide access to Good Shepherd Catholic School and the Scugog Community Recreation Centre.



Adjacent Reach Street provides paved shoulders for cycling use and connects to the nearby multi-use path east of Old Simcoe Road. In addition, GO Transit and Durham Region Transit provide bus stops on Reach Street at the Sherrington Drive intersection.

On this basis, Sherrington Drive and Chimney Hill should be considered for Bicycle Priority Street treatment. Recognizing implementation could take time, the Township should begin by posting on-road bike route signs in the interim.

Simcoe Street (north of Reach Street)

Simcoe Street (Regional Road 2) is a two-lane mostly rural Type A Arterial road from north of Reach Street to Castle Harbour Drive. The road carries a high volume of traffic, including many trucks, owing to its important function as the main gateway to Port Perry from the north. In this role, Simcoe Street connects to many places of interest in the Township.

The proposed Regional Cycling Plan Update Primary Cycling Network denotes a future facility on Simcoe Street. Given the higher volumes of vehicular traffic, a boulevard multi-use path should be considered for the east side of the road. The path would replace a short section of existing sidewalk. It is noted the Regional plan contemplates of paved shoulder as the preferred facility type.

2.2 RURAL AREA

Table 4.3 of the AT and TMP summarizes the proposed east-west and north-south cycling routes for rural areas of the Township as shown in **Map 9**. In the absence of volume data and existing facilities, the cycling network in the rural area was formed primarily from routes commonly used by cyclists currently. Directness of routing and roadway features such as gravel surfaces, steeper grades, higher volumes, and higher speeds also factored into network development. Connecting hamlets to the Port Perry Urban Area was another consideration in route selection.

As noted in **Table 4.3**, signed on-road bike routes should be implemented on all routes except for Old Simcoe Road and the proposed Second Island Access, where construction of a paved shoulder should be considered. The relatively low volumes of vehicular traffic and projected cycling use would typically not warrant the introduction of dedicated cycling facilities on these roads.

Wayfinding signs should be installed with the signed on-road bike routes in the rural area to both guide cyclists in their travels and alert motorists to potential cycling activity. The sign design should be distinctive and easily understood by users and be coordinated with the comprehensive **Active Transportation Outreach Strategy**



recommended in **Section 4.8** of the AT and TMP (Recommendation 4.10) to help promote cycling in the Township. The signs can also support tourism marketing initiatives. **Figure D.6** illustrates sample wayfinding signs for the Greenbelt Route in Ontario and the Blue Route in Nova Scotia.



FIGURE D.6: SAMPLE WAYFINDING SIGNS

3 PROPOSED SOLUTIONS TO GAPS AND DISCONTINUITIES

This section details the assessments completed to resolve remaining gaps and discontinuities in the proposed cycling network per Step 4 of the *Cycling Network Development Process* illustrated in **Figure 4.3** of the AT and ATP.

The following describes the identified gaps and discontinuities in the Port Perry Urban Area listed in **Table 4.5** of the AT and TMP:

Waterfront Trail – Boardwalk to the Rail Trail

Section 2.1 noted gaps remain in the Waterfront Trail, including a missing link between the Boardwalk and the Rail Trail. **Figure D.7** illustrates the solution considered to resolve this discontinuity. **Table D.2** summarizes the assessment of the connection, which involves:

- Connecting the Boardwalk to the sidewalk on the south side of the Scugog Memorial Public Library with a new concrete sidewalk;
- Providing a cyclist-friendly transition (i.e., curb cut) from the sidewalk onto the roadway within the roundabout; and
- Installing shared use on-road bike route signs and pavement markings (“sharrows”) within the roundabout and on Old Rail Lane to guide cyclists from the roundabout to the Rail Trail.

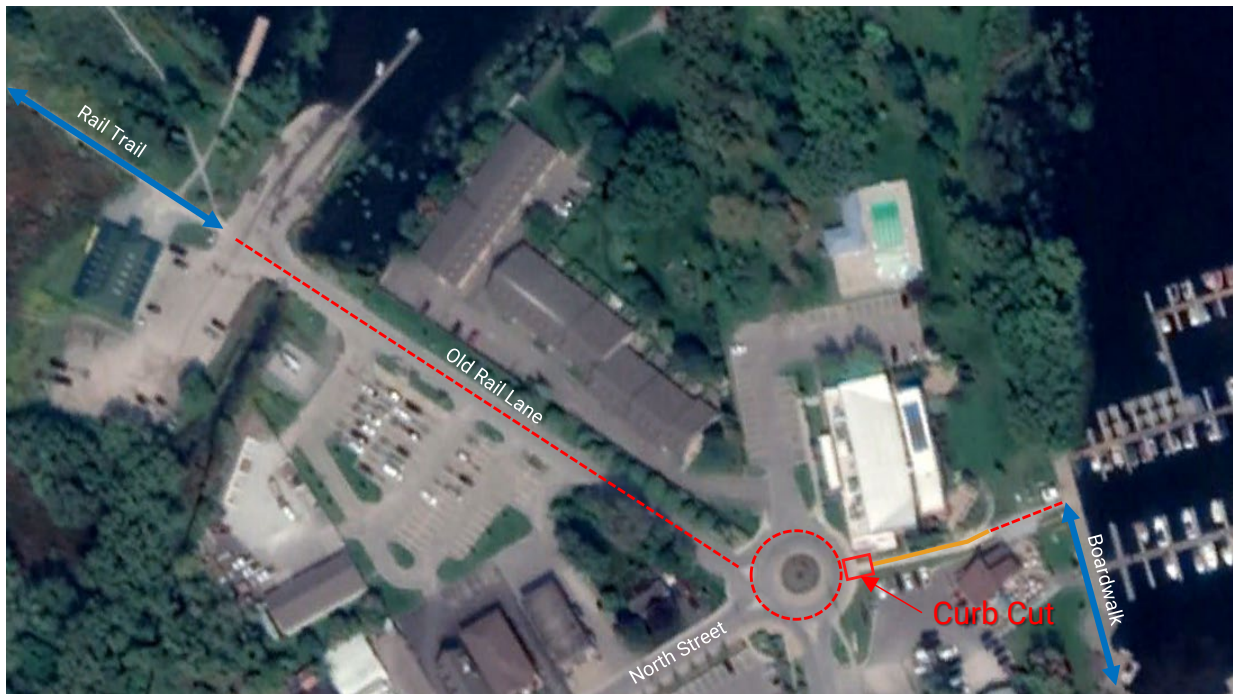


FIGURE D.7: OPTION TO CONNECT BOARDWALK TO RAIL TRAIL FOR WATERFRONT TRAIL

Waterfront Trail – Curt Street to South of Scugog Street

A second gap in the Waterfront Trail exists between Curt Street and (south of) Scugog Street. **Figure D.8** illustrates three options considered to resolve this discontinuity. **Table D.3** summarizes the assessment of the alternatives. Based on the analysis, Options 1 and 3 were identified as the preferred solutions.

Option 1 (Water Street and Carnegie Street and referred to as Alignment 1) involves:

- Coordinating with the Province to construct a multi-use path on the south side of Scugog Street from Water Street to Carnegie Street;
- Installing a crossside on Scugog Street at Water Street; and
- Constructing a multi-use path on the east side of Water Street from Curt Street to Scugog Street.



TABLE D.2: ASSESSMENT OF OPTION TO CONNECT BOARDWALK TO RAIL TRAIL FOR WATERFRONT TRAIL

Option	Description	Opportunities	Challenges
Waterfront Trail – Boardwalk to Rail Trail PREFERRED	<ul style="list-style-type: none"> Construct concrete section from boardwalk to sidewalk on Water Street Construct curb cut and ramp at new roundabout leg exclusively for cyclists Install shared use on-road bike route (“sharrows”) within roundabout and on Old Rail Line 	<ul style="list-style-type: none"> Connects two major active transportation facilities Old Rail Lane is a low volume roadway where cyclists of all ages and abilities can feel comfortable sharing the road 	<ul style="list-style-type: none"> Cyclist and vehicle interactions required within roundabout Library may limit pedestrian and cyclist sight lines at southeast corner of building creating potential for conflicts



Option 3 (New Connection and Future Multi-Use Trail and referred to as Alignment 2) involves:

- Coordinating with the Province to construct a multi-use path on the south side of Scugog Street from the commercial driveway access (Port Perry Plaza/grocery store) to the proposed future multi-use trail adjacent to the water;
- Installing a crossside on Scugog Street at the commercial driveway access (approximately 130 east of Water Street); and
- Coordinating with the commercial property owner to construct a multi-use path linking the multi-use trail on Curt Street to the crossside on Scugog Street.



FIGURE D.8: OPTIONS TO CONNECT CURT STREET TO SCUGOG STREET FOR WATERFRONT TRAIL



TABLE D.3: ASSESSMENT OF OPTIONS TO CONNECT CURT STREET TO SCUGOG STREET FOR WATERFRONT TRAIL

Option	Description	Opportunities	Challenges
<p><u>Option 1</u></p> <p>Water Street and Carnegie Street</p> <p>PREFERRED</p>	<ul style="list-style-type: none"> Coordinate with Province to construct boulevard multi-use path on Scugog Street Install signalized crossride on Scugog Street at Water Street Construct boulevard multi-use path on east side of Water Street 	<ul style="list-style-type: none"> Provides direct connection Leverages existing and planned infrastructure Separated facility provides a safer, more comfortable experience for cyclists 	<ul style="list-style-type: none"> Limited boulevard space on south side of Scugog Street Coordination required with Province on type and timing of facility on Scugog Street Several driveways along route increase potential for motorist-cyclist conflicts
<p><u>Option 2</u></p> <p>Water Street and Future Multi-Use Trail</p>	<ul style="list-style-type: none"> Coordinate with Province to construct boulevard multi-use path on Scugog Street Install signalized crossride on Scugog Street at Water Street Construct boulevard multi-use path on east side of Water Street 	<ul style="list-style-type: none"> Provides a direct connection and continues the Waterfront Trail Leverages existing and planned infrastructure Separated facility provides a safer, more comfortable experience for cyclists Does not require coordination with commercial property owner 	<ul style="list-style-type: none"> Coordination required with Province on type and timing of facility on Scugog Street Greatest length of new facility Limited boulevard space on south side of Scugog Street west of commercial driveway access



TABLE D.3: ASSESSMENT OF OPTIONS TO CONNECT CURT STREET TO SCUGOG STREET FOR WATERFRONT TRAIL

Option	Description	Opportunities	Challenges
<p><u>Option 3</u></p> <p>New Connection and Future Multi-Use Trail</p> <p>PREFERRED</p>	<ul style="list-style-type: none"> Coordinate with Province to construct boulevard multi-use path on Scugog Street Install signalized crossride on Scugog Street at commercial driveway access (Port Perry Plaza/grocery store) Coordinate with commercial property owner to construct multi-use path linking trail on Curt Street to crossride 	<ul style="list-style-type: none"> Provides a direct connection and continues the Waterfront Trail Leverages existing and planned infrastructure Separated facility provides a safer, more comfortable experience for cyclists 	<ul style="list-style-type: none"> Coordination required with Province on type and timing of facility on Scugog Street Unpredictable motorist behaviour in parking lot could increase cyclist-motorist conflicts Coordination required with commercial property owner(s) to install facility within parking lot area



Waterfront Trail – Wayfinding

Establishing a continuous, accessible trail along the shore of Lake Scugog is an important objective of the AT and TMP. Segments of the Waterfront Trail currently exist between Curt Street to Canterbury Commons with a surface type varying between crushed limestone, concrete, asphalt, and wood (boardwalk) as **Figure D.9** shows. The portion between Curt Street and the waterfront boardwalk is connected and easy to follow, but the remaining sections are disconnected and more difficult to navigate.

The text above assesses options for resolving the physical gaps in the Waterfront Trail. As a first step to improve overall navigation along the route, wayfinding signs and pavement markings (where applicable) should be installed to guide users between the different segments. In the longer term, a boulevard multi-use path on Simcoe Street should also be provided to connect the trails.



FIGURE D.9: WATERFRONT TRAIL